



# City of Tustin Preferential Permit Parking Program Policies and Procedures

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## 1 Overview

The City of Tustin's (City) Preferential Permit Parking Program (Permit Program) exists to safeguard on-street parking availability for residents and their guests. The current policy is intended to mitigate parking intrusions that may impact the quality of life or public safety in the residential neighborhoods throughout the City.

This document summarizes the City's procedures for the Permit Program. The purpose of this document is to function as a resource, providing clarification and guidelines for the Permit Program to ensure consistent, policy-driven implementation.

### Goal of the Program

The City has defined the following goals for the Permit Program to increase on-street parking availability for residents and their guests and to encourage maximum usage of off-street parking assets.

1. **Equitable Access:** Develop equitable programs that appropriately balance the parking needs of all residents and visitors, while enabling the on-street parking supply to serve the community fairly and enhance access for all.
2. **Sustainable solutions:** Implement financially sustainable strategies that modernize and streamline parking program management.
3. **Efficient Program Management:** Create an efficient and adaptable parking system that is optimized for the City's current needs but can be incrementally updated and adjusted over time.

### Definitions

- **Adjacent:** Next to, or adjoining.
- **Household / dwelling:** A residential dwelling unit (i.e., house, apartment, or condo) that per address is eligible for one vote. Includes single family and multifamily residences.
- **Parking permit districts:** Defined areas in which permit holders within a district are eligible to park on any permitted street within their district.



- **Petitioner / applicant:** The resident responsible for the petitioning and application for implementation or removal of permit parking.
- **Preferential Permit Parking Program / Permit Program:** The City's parking permit program which restricts parking from 2 AM – 6 AM, 7 days a week, on permitted streets, except by permit.
- **Eligibility to vote:** Each household/address within the permit parking petition area is eligible for one vote.
- **Permit Parking Petition Area:** A street or series of contiguous streets, either single or multi-family households within a permit parking district that are included in the permit parking petition area. The City has the authority to define and/or amend the petitioned area.



## 2 Establishing Permit Parking (Public Works Department)

This section outlines the steps to request permit parking.

1. **Petitioner / applicant contacts Public Works Department (“City”) to request permit parking.**
  - To request a new permit parking area, not adjacent to a current permitted area the petitioner must gather signatures from a minimum of four (4) contiguous streets or multi-family complex with street frontage with a total of 65 percent or more of households in support of implementing a new preferential permit parking restriction to ensure the majority interest in establishing permit restrictions.
  - To request a new permit parking area adjacent to a current permitted area, the petitioner must gather signatures from the petitioned area with a total of 65 percent or more of households in support of implementing a new preferential permit parking restriction to ensure the majority interest in establishing permit restrictions.
  - A petition form with households eligible for a vote in the petitioned area will be available to the petitioner within two (2) weeks of application submittal.
  - The City has the authority to define and/or amend the petitioned area.
  - A non-refundable \$500 fee (or \$50 limit per household) must accompany the submitted petition to off-set City costs associated with the processing of the petition.
  - Residents participate in the survey (i.e., one vote per household).
2. **Following the petition submission, City staff will verify the signatures submitted.**
  - If the City verifies that there are 65% or more households in favor of implementing permit parking, the new permit area will be approved.
  - Signatures must be clear and legible for verification purposes. Signatures which are not able to be verified will not count towards the petition.
  - If it is found that less than 65% of households are in favor of implementing permit parking, the area cannot resubmit an application for two years, at which time a new petition and application fee must be submitted for consideration.
  - The City will review applications and petition submissions on a first-come-first-serve basis.
3. **The City will notify residents of the results of the application.**
  - If the permit area has been approved, the notification to residents in the petition area will contain next steps.

**4. Upon approval, the City will install the necessary permit signage and residents in the petition area will be able to purchase permits.**

*Once an application and petition have been submitted, it can take up to three (3) months for review and results. If approved, the time to set up the new permit area can take up to three (3) additional months.*



*Figure 1. Process for establishing new permit parking.*

**5. How to remove a parking permit designation**

- In order to remove a parking permit designation, a petition shall be submitted with 65 percent of households in the permitted area in support of removing the restriction for it to be considered.
- In deciding to remove parking permit designation from a single street, City staff will carefully consider the surrounding area and permit district. A street where most of the surrounding streets (50 percent or more) are included in the permit district might not be considered for removal because of the spillover parking risk.

**6. The City (Police and Public Works / Engineering Department) will continue to monitor parking occupancy and program effectiveness.**

- The City will monitor the program and will consider policy changes if, for example, an established permit district is found to regularly reach or exceed 85 percent occupancy.

### 3 Parking Permit Districts

The City will define the parking permit districts. Permit holders within a district are eligible to park on any permitted street in their district which includes the streets in the recently approved permit parking petition area in addition to streets already designated as permit parking. The defined permit districts take into consideration the number of eligible households along with the amount of available street parking. The goal of this permit district system is to cover a whole neighborhood or subarea, including both single and multi-family homes, and generally provide parking opportunities within a reasonable walking distance.

Figure 2 shows the conceptual map of a parking permit district.

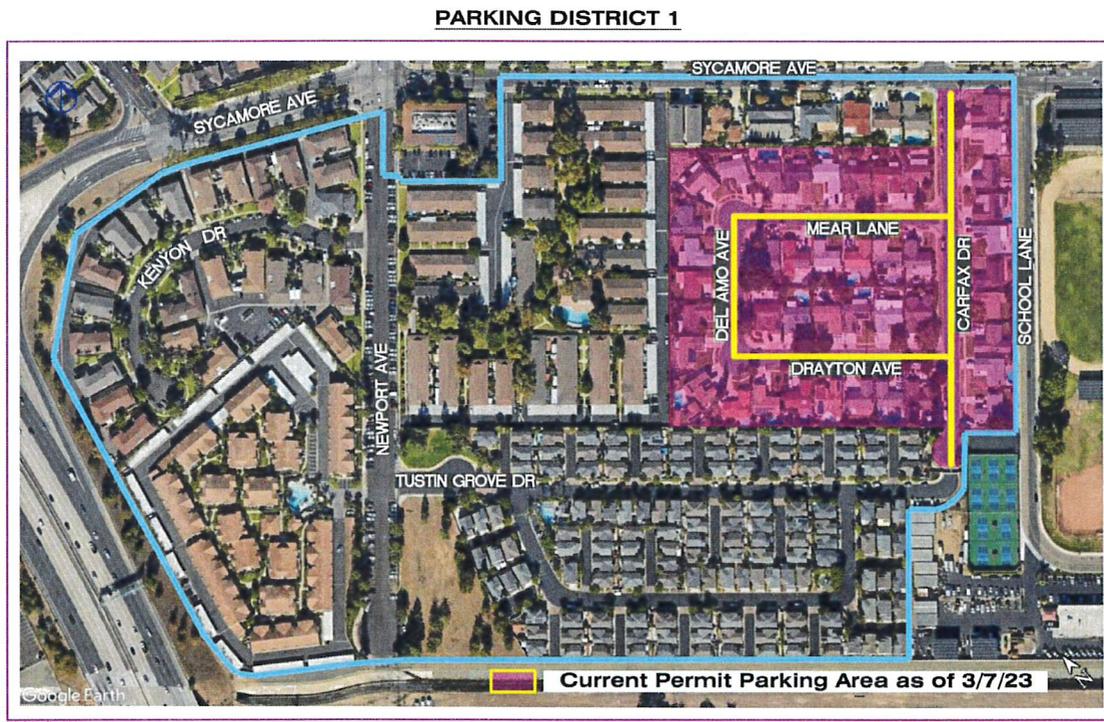


Figure 2. Parking Permit Map

## Purchasing Permits

- A parking permit is \$25.00 for the first preferential parking permit per household. There is an additional escalating premium of \$25.00 per additional permit sold where permit two would be \$50.00, permit three would be \$75.00, and permit four would be \$100.00 (see Figure 3). Each permit purchased is associated with a single vehicle and license plate that meets the City's eligibility requirements.
- Each household in a permitted area is eligible for 75 guest permits (i.e., 75 days) per 6-month period. There is no charge for guest permits. To obtain a guest permit, the resident must have the vehicle information including the license plate number. A vehicle that is already assigned a permit is not eligible for a guest pass.
- Permits are virtual, meaning there is no physical permit, your license plate number is the unique identifier associated with your vehicle.
- Fees are reviewed annually and included in the City's Comprehensive Schedule of Fees.
- Parking permits must be obtained online via a third-party vendor.
- The permit is valid for an annual period between February 1<sup>st</sup> and January 31<sup>st</sup>.



*Figure 3. Escalating Parking Permit Fees*

## Replacements

Because the permits are virtual, there is no need to replace a lost or stolen hangtag. Permits are non-transferable except in the case of a resident getting a new vehicle.

## Revoking Permit Parking Privileges

All Parking Permits remain the property of the City of Tustin and may be revoked without notice if used contrary to the provisions of this policy.





## **5 Good Neighbor Policies**

Residents applying for parking permits will be required to sign a Good Neighbor Policy.

These good neighbor policies help guide neighborhood ethos, promote friendly and cooperative interactions, and soothe residential “parking wars.”

The Good Neighbor Policy includes, but is not limited, to the following:

- “Off-street, on-site parking supply shall be used efficiently for parking. Examples of nonefficient use include not utilizing garages and driveways for parking.”
- “Permit parkers shall be considerate of noise and comply with applicable city noise ordinances.”
- “Permit parkers shall not move solid waste containers or set out cones in a manner that precludes collection of solid waste, obstructs driveways or other rights of way, or otherwise interferes with vehicular traffic in order to park on a street or portion thereof designated as permit parking only.”

Residents will be required to sign these policies when applying for a permit. This is a valuable reminder that public streets are there to be used by all residents while encouraging all to be courteous and mindful of our neighbors.